



Administrative Design Review Standards Manual

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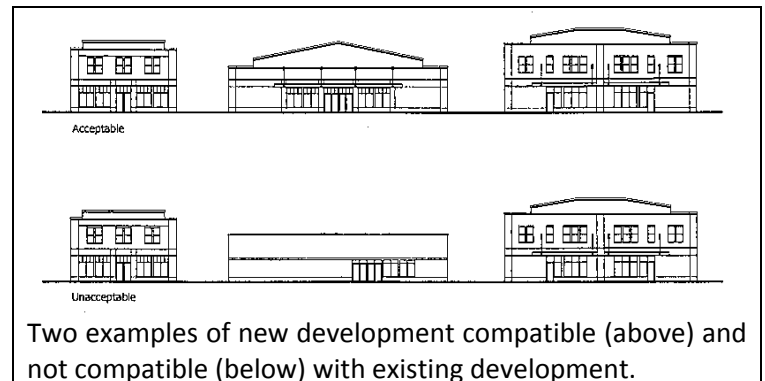
Introduction

This manual serves to familiarize applicants with the Village's Administrative Design Review Standards adopted by Village Council on January 14, 2010 through Ordinance No. 10-01. To further inform applicants of the standards, a Pre-Application Conference is required prior to submitting applications for development permits that would trigger Design Review. Refer to Section 30-215(d)(20) of the Code of Ordinances.

What is Design Review?

Design Review is a strategy for managing the details of new development in an efficient and timely manner. The focus of Design Review is on the physical appearance of new construction and site planning details, as well as traditional planning concerns, such as landscaping, parking, traffic, signage, and lighting. Design Review nurtures community character by holding developers to measurable design and architectural principles. Design Review processes should not employ strict, narrowly focused controls. Rather, they should provide a framework that helps ensure equity and consistency in the approval of new development, while allowing flexibility and creativity.

Ideally, Design Review encourages development that enhances community character and ensures new development will be compatible with the existing neighborhood.



Why Design Review?

The purpose and intent for Design Review is to:

1. Enhance Islamorada's built environment, reinforce its existing community character and protect its historic character.
2. Encourage new construction that compliments and is consistent with Islamorada's character.
3. Have a set of Design Review Standards that are fair and equitable while maintaining consistency with the goals, objectives and policies of the Comprehensive Plan.
4. Ensure the standards are administratively-practical, user-friendly, understandable, predictable while also being flexible, allowing for creativity and not causing a financial burden.
5. Promote safety and minimize conflicts between vehicles and pedestrians.
6. Be business-friendly and sensitive to parking and visibility concerns.

Where Does Design Review Apply?

Design Review applies to property with frontage on any major street (e.g., Overseas Highway, Old Highway) and all property within the Village Center (VC) zoning district.

What Triggers Design Review?

Design Review applies to nonresidential development projects, which include the development of new structures and substantial improvement of existing structures, subject to Site Plan or Conditional Use review pursuant to Chapter 30, *Land Development Regulations (LDRs)*, of the Code of Ordinances.

The following are examples of development that would trigger Design Review:

- 1) Low/medium intensity office, retail and service uses greater than 3,000 square feet in the Village Center (VC) and Highway Commercial (HC) Zoning Districts or greater than 2,500 square feet in the Neighborhood Commercial (NC) Zoning District such as a bakery, bookstore, clothing store, delicatessen, department store, drugstore, electronics store, florist, food store, gym, hardware store, jewelry store, liquor store, movie theater, office supplies, salon, spa, video rental and walk-up bank.
- 2) Hotels or motels having more than 25 rooms.
- 3) Restaurants greater than 1,500 square feet.
- 4) Bars, taverns and drinking places.
- 5) Outdoor storage and display areas.
- 6) Any use with a drive-through or drive-in component.
- 7) Substantial improvement (repair or reconstruction of a building which exceeds 50% of its value) of an existing structure.

The following are examples of development that would not trigger Design Review:

- 1) Changes of sign copy.
- 2) Permitted (“as-of-right”) low/medium intensity office, retail and service uses less than 3,000 square feet in the Village Center (VC) and Highway Commercial (HC) Zoning Districts or less than 2,500 square feet in the Neighborhood Commercial (NC) Zoning District. Permitted uses vary by zoning district.
- 3) Restaurants less than 1,500 square feet [except restaurants in the Commercial Fishing (CF), Marine Use (MR) and Neighborhood Commercial (NC) Zoning Districts].
- 4) Non-substantial improvements (repair or reconstruction of a building which is less than 50% of its value) of an existing structure with a permitted use—such as interior renovations, re-roofing, window and door replacements, etc.
- 5) Residential development projects without a commercial use.
- 6) Minor corrections to approved site plans such as a change in sign location, minor modifications to parking areas, relocation of terminal islands to accommodate trees or utility lines, reduction in building footprint size, addition of small canopies, removal of excess parking for additional open space and changes of sign copy.

Who Reviews and Approves Design Review Projects?

Design Review is part of the development review process as set forth in the LDRs and is reviewed by Village staff, the Development Review Committee and approved by the Director of Planning and Development Services (the “Director”). No Village Council approval is necessary unless a public hearing is expressly required (e.g. for Major Conditional Uses and structures over 10,000 square feet) or requested (e.g. Site Plan Approval and Minor Conditional Uses) for a Development Permit as set forth in the LDRs. Public notice is the same as required by the Code for the applicable development permits.

ADMINISTRATIVE DESIGN REVIEW STANDARDS pursuant to Code Section 30-215(d)(20).

Roof Pitch

The maximum roof pitch of all street-facing facades on principal buildings is 8/12, expressed as rise over run. Flat roofs are allowed provided they are hidden by a parapet or other architectural feature in a style consistent with the building design as approved by the Director and any roof-top utilities or equipment are screened from view from all adjacent streets.



An example of a building with an 8/12 roof pitch.

Awnings

Awnings cannot be covered with any transparent, translucent, high gloss or high sheen material and cannot be backlit. Awnings may be covered with any other material allowed by Village Code.

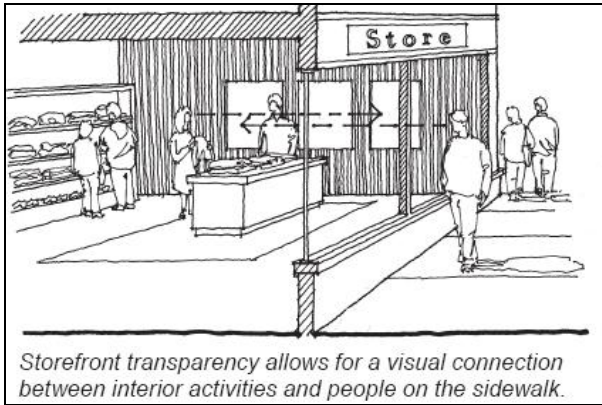


Examples of backlit (left) and translucent (right) awnings.

Building Transparency

In order to create a visual connection between a building's interior activities or retail display windows and people outside of a building, the minimum transparency of the lowest floor of all principal buildings is 35 percent per each 50 linear feet of each street-facing facade. To count as transparency, windows, retail display windows and doors cannot be opaque or tinted with an inside-to-outside light transmittance value of 45 percent or less. Windows may be operable or non-operable. Doors must be operable and may include emergency egress doors. All required transparency must be evenly distributed at pedestrian level across the length of the lowest floor. Transparency is not prohibited from ex-

tending higher than seven feet above the bottom of the lowest floor, however the measurement to determine whether this standard is met is between zero feet and seven feet in height above the bottom of the lowest floor.



Left: Diagram illustrating the “transparency” standard. Right: A good example of transparency.

Compliance with the above standard is mandatory, unless the director finds that it is impracticable for all or a portion of a principal building, in which case the development must comply with one of the following standards:

1. A professional mural, which constitutes art as defined in the LDRs, must be painted on the entire façade of each street-facing side of a building.



Example of a building that effectively used murals to create the effect of transparency.

2. Foundation landscaping must be installed consisting of a Class G bufferyard pursuant to division 6 of article V of the LDRs and located entirely within 15 feet of a building.



Left: Diagram of a Class G bufferyard, which is ten feet wide and consists of two canopy trees, five understory trees and 40 shrubs per 100 linear feet.



Examples of foundation landscaping.

3. Any combination of the above standards, which results in the entire façade of each street-facing side a building having foundation landscaping where no mural is painted.

Building Articulation

The facades and roofs of all principal buildings must be articulated through the following techniques:

1. Facades and roofs must have no less than one articulation every 50 linear feet.
2. Minimum horizontal façade articulation is five feet in depth and 15 feet in width.
3. Building facades must feature some combination of the following elements: arcades, awnings, balconies, canopies, porches, decks, bay windows, recessed entries, retail display windows or other similar features as approved by the Director.
4. Roof articulation must be achieved through the use of cornices, dormers, eaves, gables, multi-planed roofs, offsets, parapets, pitched roofs, roof overhangs or other similar features as approved by the Director.
5. Flat roofs within view from an adjacent street must be hidden by a parapet or other architectural feature in a style consistent with the building design as approved by the Director and of a sufficient height to screen any rooftop utilities.
6. A parapet on a flat roof cannot be flat and must feature cornice treatments.
7. Boat barns are exempt from this standard.



Examples of building articulations that would be permitted (left) and not permitted (right).

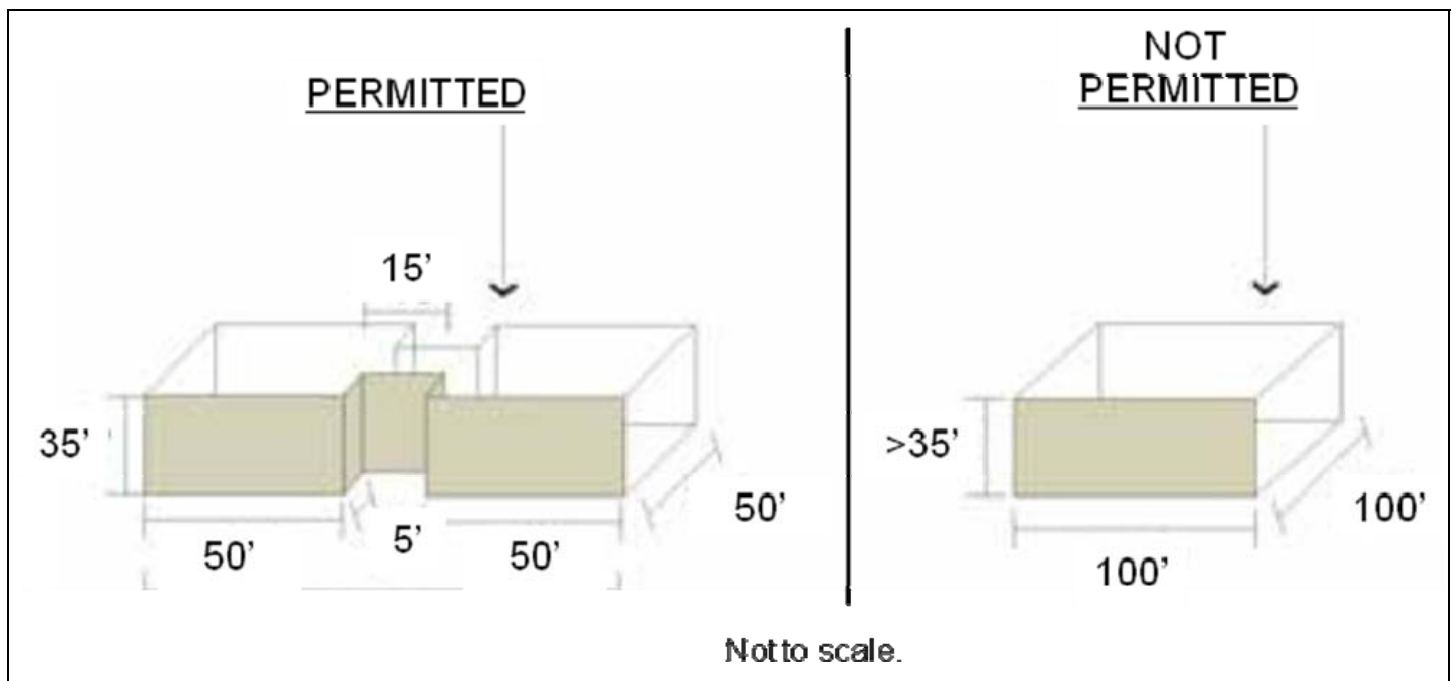
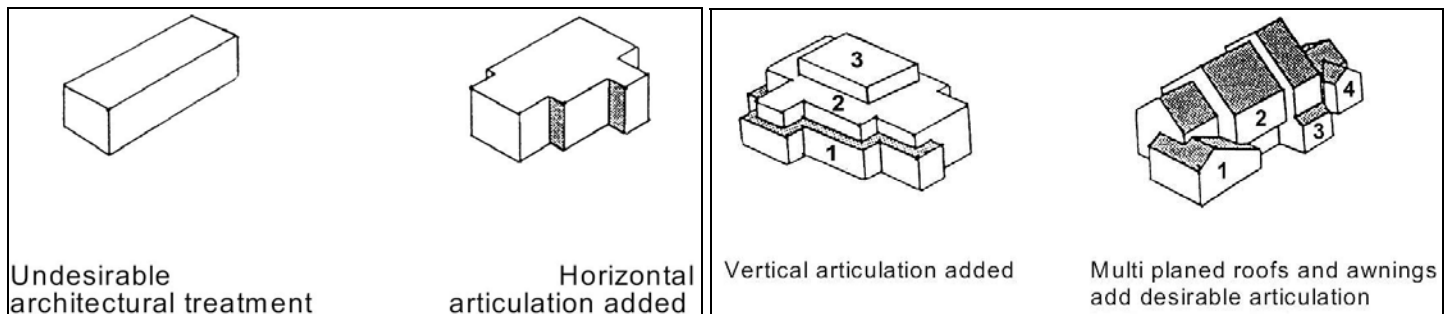


Diagram of façade articulations that would be permitted and not permitted.



Diagrams of façade and roof articulation ranging from no articulation (left) to horizontal/façade articulation (middle left), vertical/roof articulation (middle right) and both (right).

Building Orientation

Each nonresidential or multi-family residential building must provide an entry that is clearly identifiable from the street, but the entry does not necessarily have to face the street. This must be accomplished through the architectural design of the building, the use of sidewalks, signage, pavement markings and/or landscaping.



Examples of buildings in Islamorada with clearly-identifiable entrances from the street.

Drive-Throughs and Drive-Ins

Any drive-through or drive-in components (e.g., for banks, restaurants, convenience or liquor stores, etc.) must be architecturally compatible with on-site principal structures and shall be located behind or to the side of the street-facing façade of a building. Drive-through or drive-in components must be designed to be located as far as possible from adjacent residential zoning districts and residential uses. In order to protect the public interest and surrounding land uses, the Director or Village Council, as applicable, may impose conditions such as the installation of landscaping and screening walls, and limiting the hours of operation.



Example of a bank drive-through located to the side of the front of the main building. Photo taken from street level (left) and an aerial photo (right) of the same property.

Car Washes

Openings, bays and entry/exit doors of all car washes must be oriented away from adjacent residential areas and streets. If oriented to a street, then openings, bays or entry/exit doors shall be screened with an opaque structure and/or landscaping. In order to protect the public interest and surrounding land uses, the director or village council, as applicable, may impose conditions such as the installation of landscaping and screening walls, limiting the hours of operation, restricting the number and location of driveways, prohibiting the use of outdoor speakers, locating blowers and dryers as far away from residential areas as possible, and limiting the hours of operation.

Utilities

All rooftop utilities or mechanical equipment must be completely screened on all sides in a manner consistent with the architectural design of the building.

(See examples on next page.)



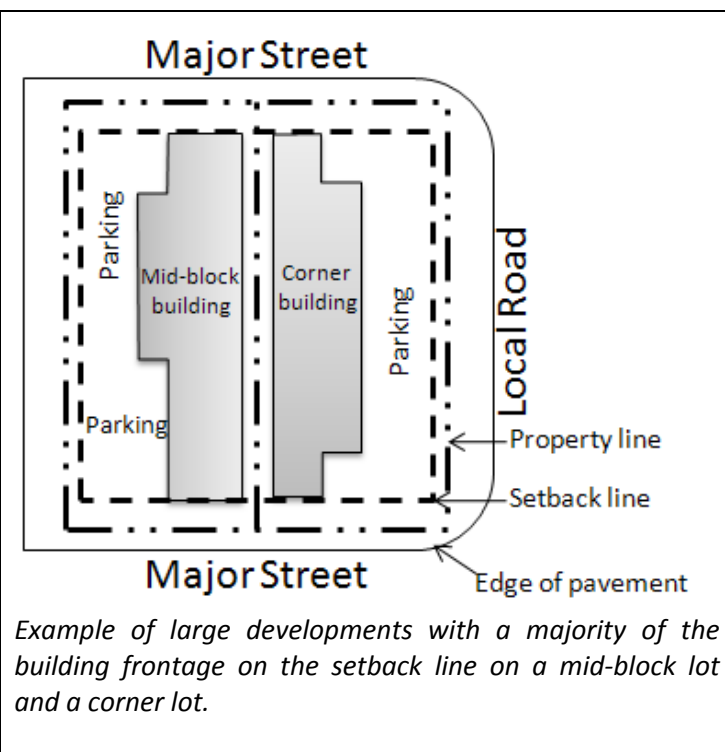
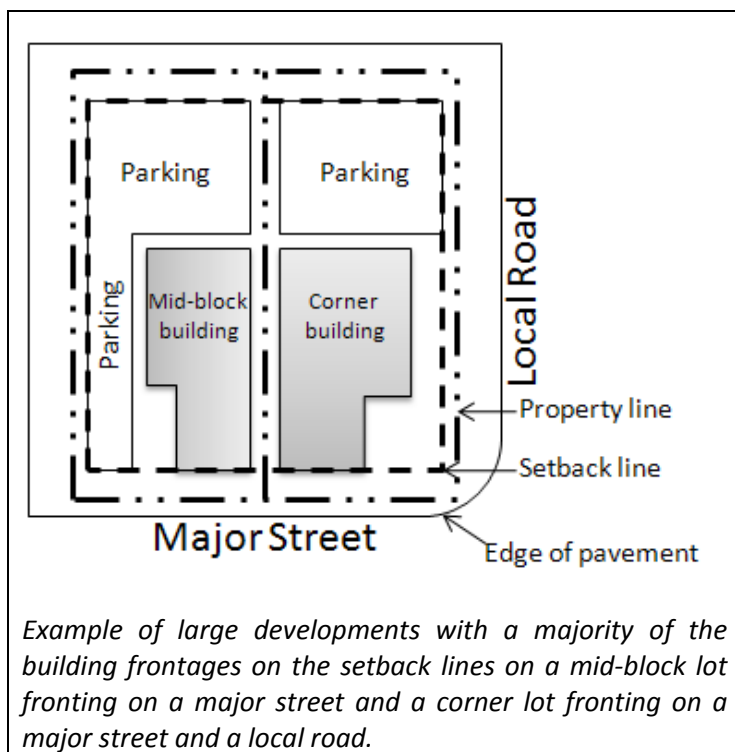
Example of rooftop equipment that is clearly visible from the ground located along the edge of the building.



Fig. 1 shows utilities located on a roof and properly screened by parapet walls. Fig. 2 shows the effectiveness of that screening from the ground.

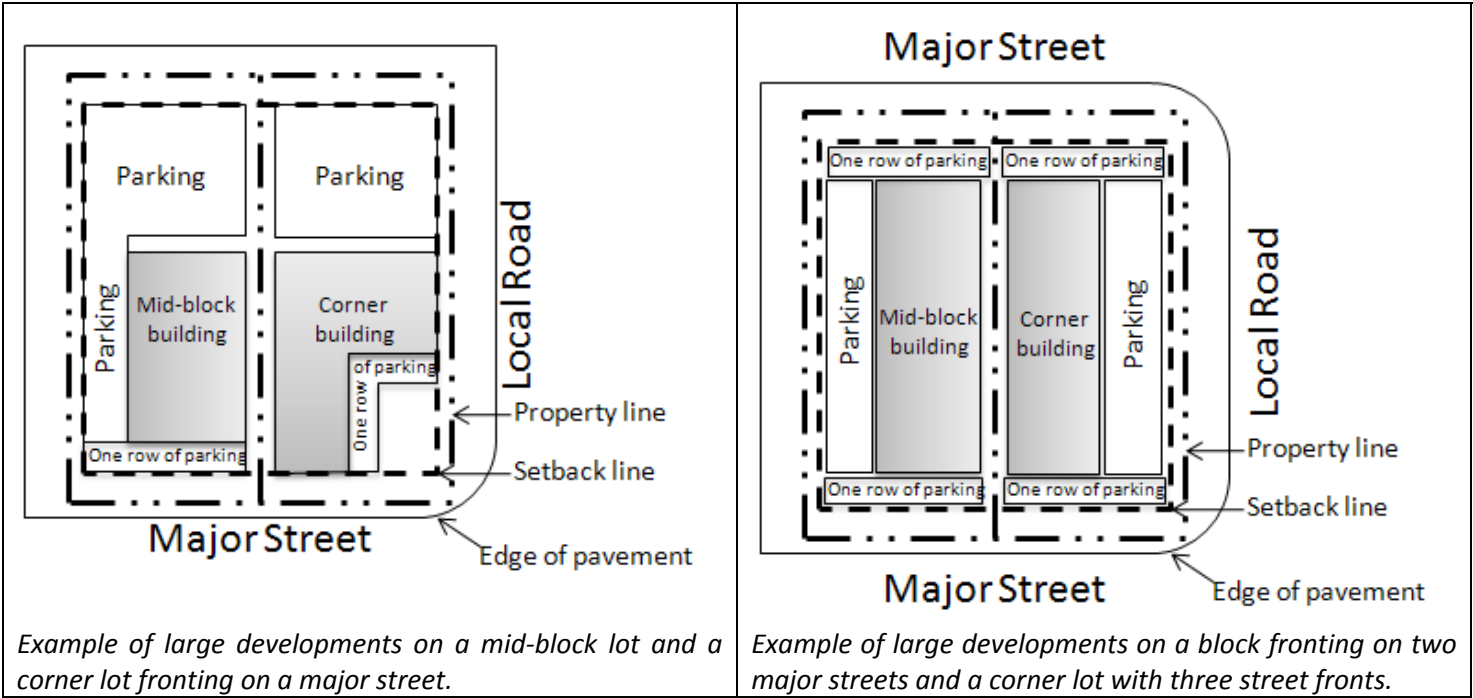
Building Location

For development 10,000 cumulative square feet in gross floor area or greater on one development site, majority (e.g., more than 50%) of the façade of each principal building must be located on the setback line along each major street of the development site.

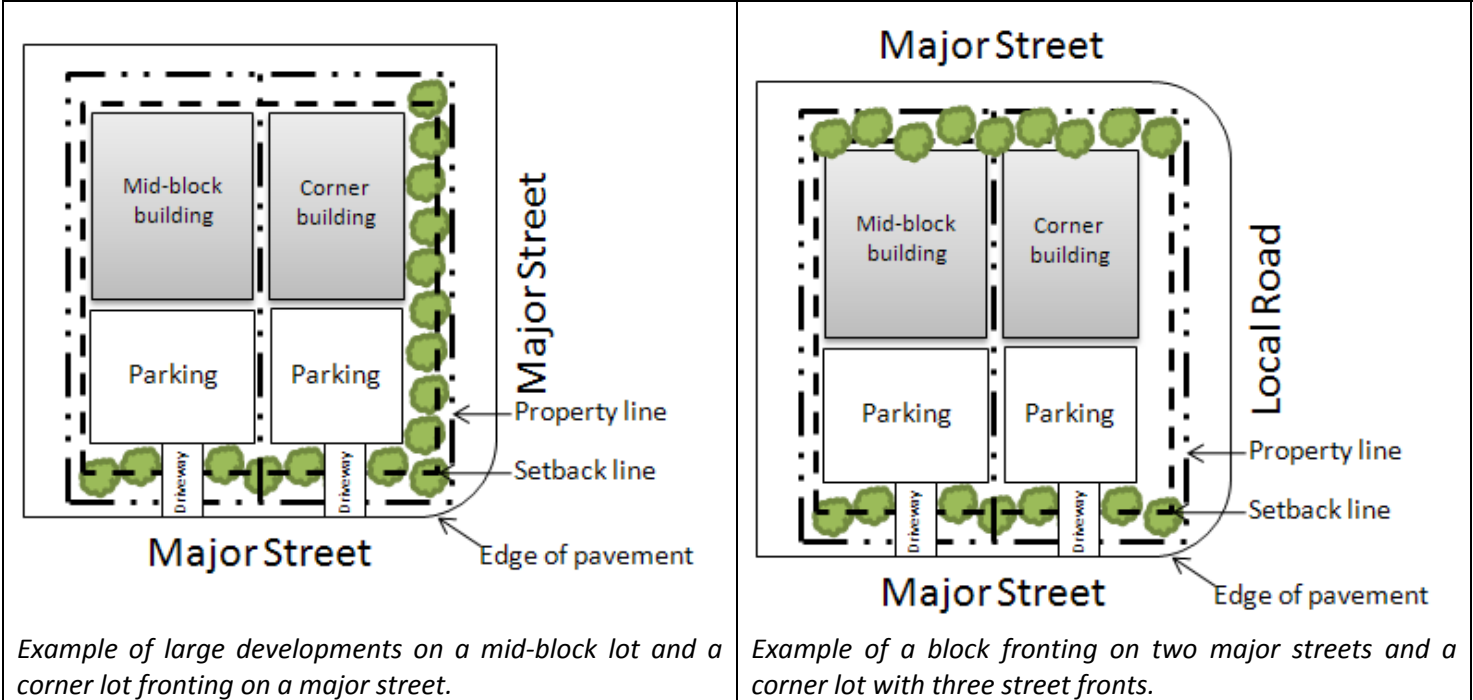


Alternatively, if less than a majority (or none) of the façade of each principal building is located on the setback line along each street of the development site, then one of the following alternative standards must be met:

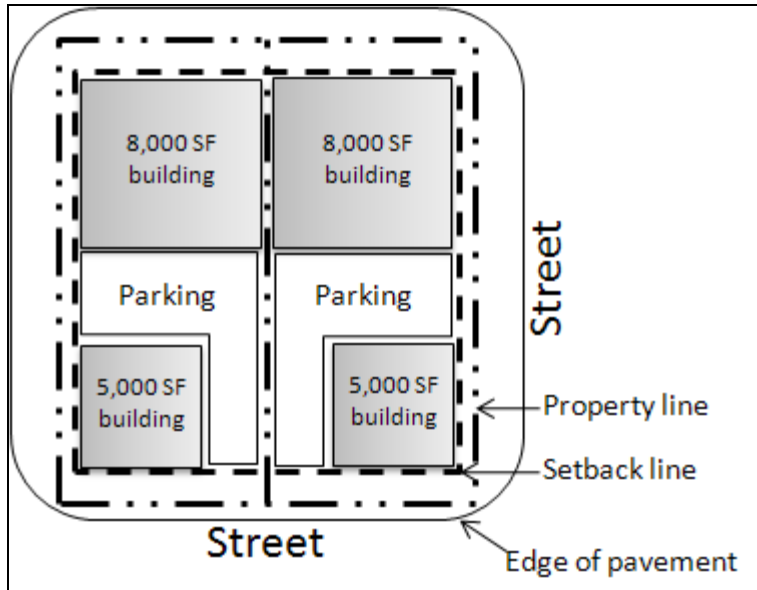
1. Off-street parking located between principal buildings and any major street is limited to one row; or



2. A scenic corridor pursuant to table 30-814 of the LDRs must be installed along all major streets. All other applicable landscaping standards still apply; or



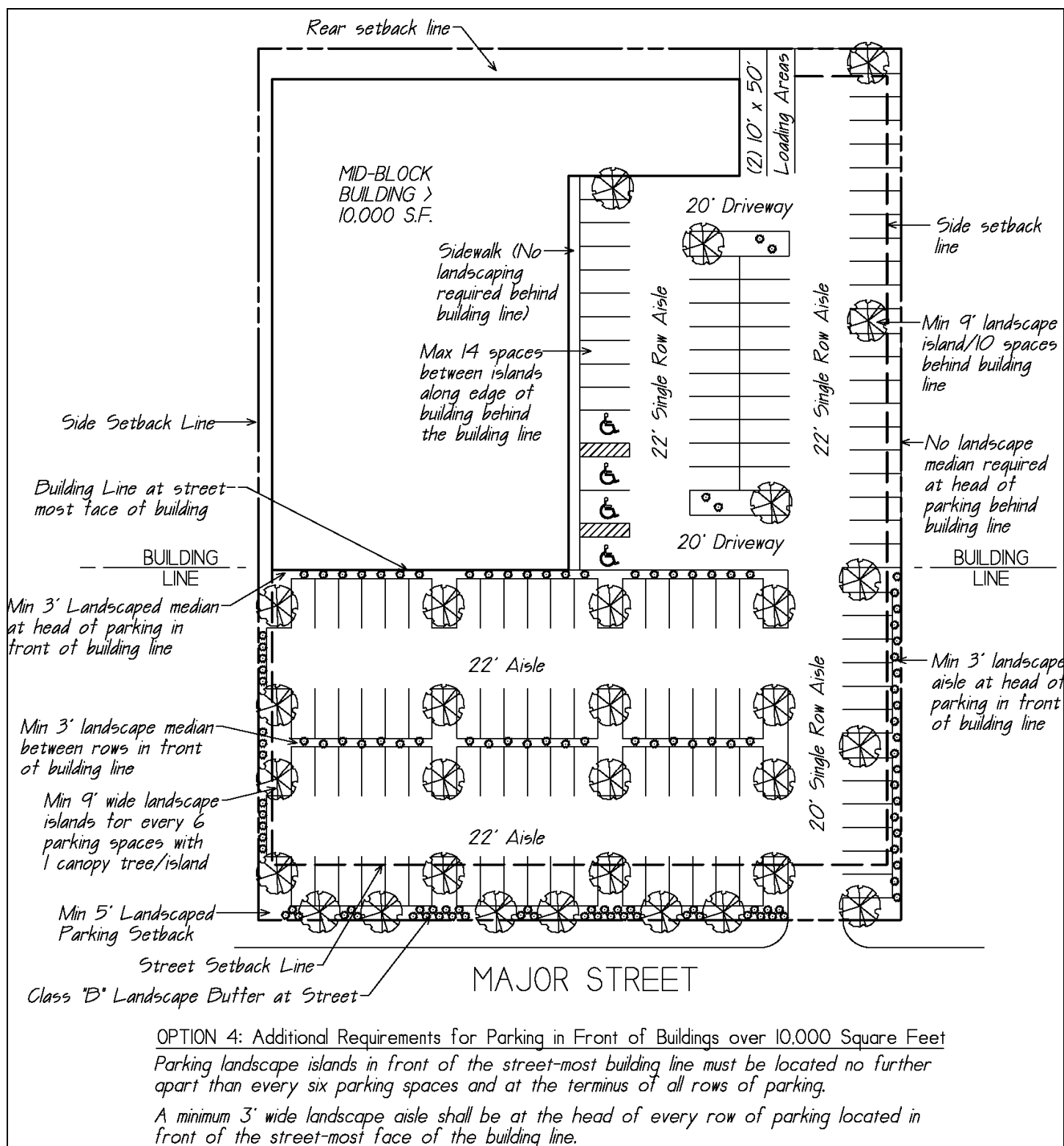
3. The development must be divided up so that no single building exceeds 10,000 square feet in floor area; or



4. Landscaping for off-street parking areas located between the street-facing facades of principal buildings and any major streets must be provided as follows:
 - a. Parking landscape islands must be located no further apart than every six parking spaces and at the terminus of all rows of parking.
 - b. Minimum three-foot-wide landscape strips must be provided at the front of rows of parking spaces and between rows of parking spaces. A minimum of one native shrub per eight linear feet is required within the landscape strips.
 - c. Landscaping for the remainder of the off-street parking areas must be provided as set forth in section 30-818 of the LDRs.

These landscaping requirements are in addition to the requirements set forth in section 30-818 of the LDRs.

(See diagram on next page.)



Development less than 10,000 cumulative square feet in floor area on one development site does not have to meet the Building Location standard. The goal of this standard is to avoid the “big box” appearance of larger developments with a “sea of parking” visible from a street.

At least one continuous pedestrian walkway must be provided from the main building entry to the public sidewalk, bike path or street edge of pavement. The walkway must be at least five feet wide and must meet all applicable ADA Accessibility Guidelines.



Example of two buildings: one with a walkway to the street (left) and one without a walkway to the street (right).

Vehicle Access

1. When practical, access to off-street parking (e.g., driveways) must be made from a collector road or a local road (e.g. De Leon Avenue, Ocean Lane, Dogwood Lane, Blackwood Drive) rather than a major street (e.g. Overseas Highway or Old Highway). For example, adjacent to Overseas or Old Highway and a local road, access to off-street parking must be from the local road. The Director or Village Council, as applicable, has the discretion to determine the practicality of access locations and to exempt certain high-intensity uses such as gas stations from meeting this standard due to the high volume of traffic typically associated with them, particularly if they would be located adjacent to residential zoning or uses.

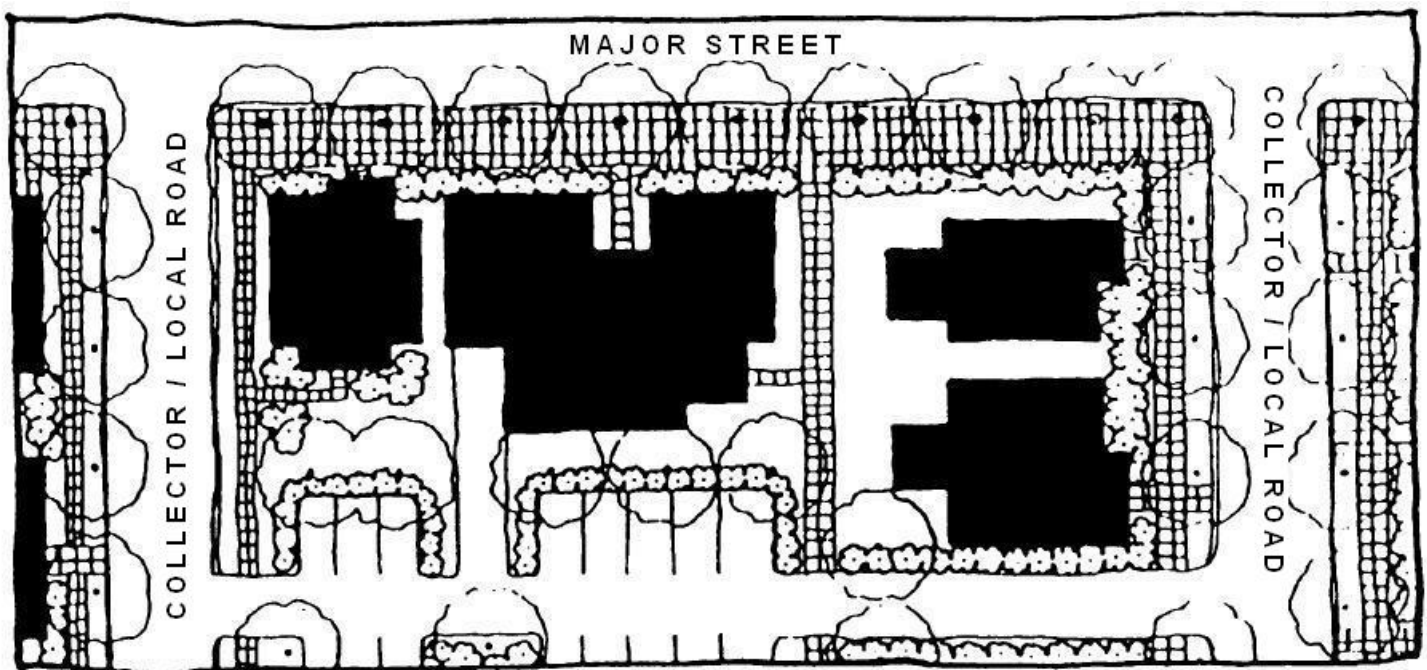
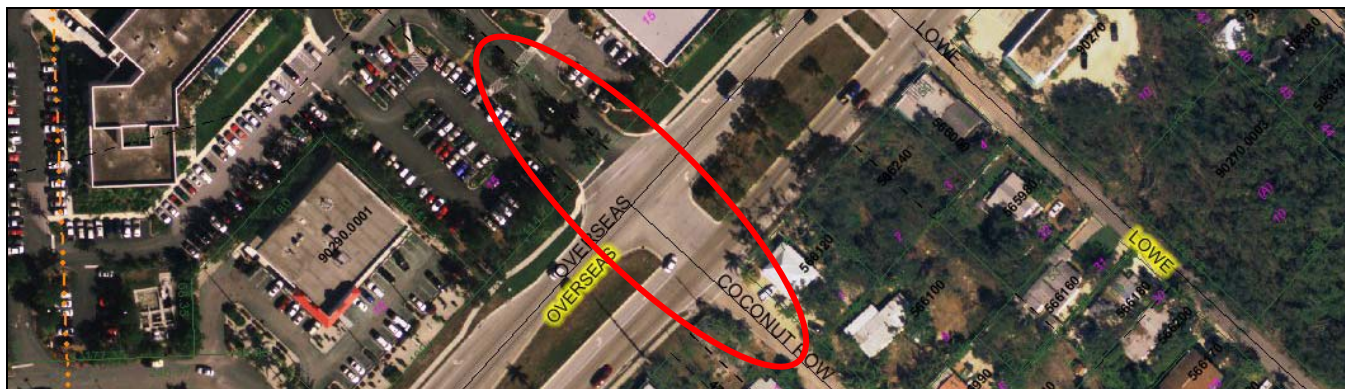


Diagram illustrating driveways that connect to secondary streets rather than a major street.

2. When practical, access to off-street parking (e.g., driveways) on property adjacent to both Overseas Highway and Old Highway must be made from Overseas Highway. This standard helps implement Comprehensive Plan Policy 1-1.2.3, which states that the Village “shall promote land use and transpor-

tation strategies on the Old Road that enhance its safe use as a local traffic and pedestrian and cyclist friendly corridor.” The Director or Village Council, as applicable, has the discretion to determine the practicality of access locations.

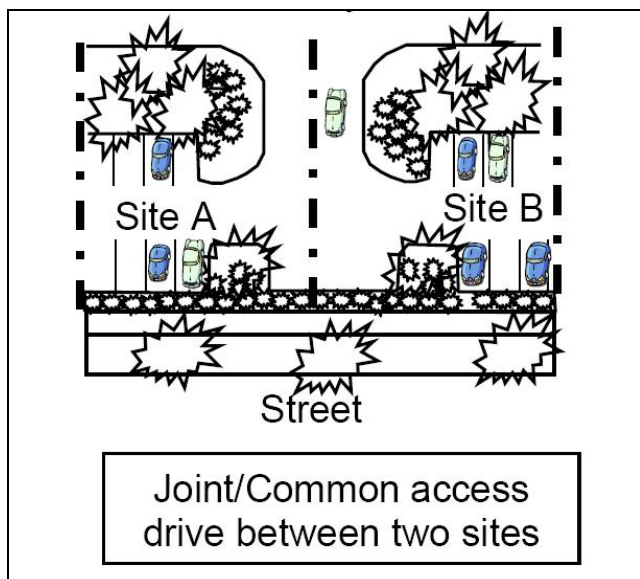
3. When practical, new driveways must be aligned with existing driveways or streets adjacent to the property. The Director or Village Council, as applicable, has the discretion to determine the practicality of access locations.



Example: A new driveway to the property to the north was located directly across from Coconut Row, an existing street.

Cross Access and Parking

When practical, adjacent off-street parking areas should be shared or interconnected to provide joint or common access between two properties. This would help reduce the need to drive onto streets to travel to next-door businesses and reduce traffic congestion caused by turning vehicles.



Oversized Vehicle Parking

To help encourage oversized vehicles not to park in the right-of-way, additional off-street parking spaces are required specifically for oversized vehicles, such as buses, cars towing boats or trailers, recreational vehicles (RVs), travel trailers and similarly large vehicles, as follows:

1. On property developed or proposed to be developed with a drive-in or drive-through component and more than ten, but less than 50 required off-street parking spaces, at least one oversized vehicle parking space must be provided.
2. On property developed or proposed to be developed with either 10,000 square feet or more of gross floor area and/or 50 or more required off-street parking spaces, at least two oversized vehicle parking spaces must be provided.
3. All oversized vehicle parking spaces must have a minimum length of 40 feet with a minimum width of ten feet.

Multi-use paths and bicycle paths

On property served by the Florida Keys Overseas Heritage Trail multi-use path or an existing Village designated bicycle path, applicants shall be responsible for making one-time improvements to the path, including installation of standard “Share the Road” or “Bike Route” signage, pavement markings, striping, colored asphalt and widening to a minimum 10-foot standard width where sufficient right-of-way and clearance exists. This requirement is subject to the applicant obtaining all necessary permits from and shall coordinate with the Florida Department of Transportation, the Florida Department of Environmental Protection and the Village Public Works Department, as applicable.



Example of faded pavement striping along the Florida Keys Overseas Heritage Trail on U.S. 1.

Newspaper Boxes

All newspaper boxes visible from any street must be screened or enclosed on three sides, leaving the front dispensers accessible. The screen or enclosure material must be architecturally compatible with adjacent buildings and may be of any material allowed by the LDRs, except chain link fences. It is the responsibility of the property owner, the newspaper box owner and/or the newsrack owner to ensure that all newspaper boxes, newsracks, screens and enclosures are maintained so as to present a neat, clean appearance, kept in safe and good working order.



Example of newspaper box screening.

Definitions

Articulation means horizontal and vertical architectural projections and recesses, such as arcades, awnings, balconies, canopies, porches, decks, bay windows, recessed entries and retail display windows on the façade of a building and varying roof forms such as cornices, dormers, eaves, gables, multi-planed roofs, offsets, parapets, pitched roofs and roof overhangs, which are intended to break up buildings into smaller identifiable pieces.

Foundation landscaping means a landscaping area located along or adjacent to each street-facing front, side or rear of a building, provided, however, where a portion of the front, side or rear of a building is devoted to pedestrian ingress/egress, vehicular ingress/egress, loading or drop-off zones, foundation landscaping areas may be aggregated into one or more locations along or abutting such front, side or rear of a building.

Newspaper box means any type of outdoor unmanned device, whether a self-service or coin-operated box, container, storage unit, newsrack or other dispenser, installed, used or maintained for the display, sale, or vending of or free distribution of newspapers, news periodicals, magazines, trade publications or journals, advertising inserts, circulars, directories, maps, digests, tabloids, broadsheets, real estate publications or any other printed materials.

Retail display window means a window or opening in the exterior wall, a display case or a window box of any portion of a building used for business purposes, through which merchandise, services, or business are displayed or advertized.

Street means all major streets, the major road network system and all local roads.

Transparency means architectural elements that fill openings in a building façade, such as transparent windows, doors and retail display windows, designed to create a visual connection between the interior activities of a building and people outside of a building, and also expressed as a percentage of a building's façade.